

ORDINANCE 97-00128

AN ORDINANCE ADOPTING STANDARDS FOR DRIVEWAY ENTRANCES.

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF MUSTANG RIDGE:

That the City of Mustang Ridge hereby adopts the standards established by Travis County, Texas as to driveway entrances, said standards being attached to and made a part of this ordinance by reference for all relevant purposes.

That a permit fee of \$25.00 shall be paid by any citizen seeking city approval of a driveway entrance for which the attached standards are applicable, said fee being subject to change upon the majority vote of the city council.

To the extent of any conflict with any prior ordinance of the City of Mustang Ridge, or the terms and provisions of the attached standards, the terms of this Ordinance shall be controlling.

If any part of this ordinance or its application is for any reason held to be unconstitutional, invalid or unenforceable, the unconstitutionality, invalidity, or unenforceability of any such part shall in no way affect, impair, or invalidate the remaining parts of this ordinance, but as to such remaining parts, this ordinance shall remain in full effect.

An action to enforce this Ordinance may be brought by the City of Mustang Ridge or by any resident of the City, with the relief therein sought including, but not limited to, damages, declaratory relief, costs of court, attorney's fees, and injunctive relief of a temporary and/or permanent nature. In addition, and without waiver of the foregoing, any person found in violation of this Ordinance shall be deemed guilty of a misdemeanor and, upon conviction, shall be subject to a fine not to exceed two hundred dollars (\$200.00). Each day of such violation shall constitute a separate offense.

Because prompt action is in the public interest, this ordinance shall be effective immediately upon its passage.

PASSED AND APPROVED this 10th day of March, 1997.

ATTEST:

Carolyn Vallejo
City Clerk

Alfred Vallejo, II
Alfred Vallejo, II, Mayor

Driveway Entrance Standards1. Application of Driveway Entrance Standards

The following driveway entrance standards are **required** for construction for driveways on public right-of-way in subdivisions constructed in Travis County outside incorporated areas. These standards are also **recommended** for driveway construction on public right-of-way in unincorporated portions of Travis County which is no in subdivisions.

2. Driveway Entrance Approval Procedure2.1 General:

Approval must be obtained for driveway entrance installation where the driveway entrance standards are required. Application is made and approval is obtained thru the City Office, concurrent with Development Permits required by "The Travis County Regulations for Flood Plain Management."

Where application is made thru the City Office, all data will be forwarded to the City Office.

2.2 Application Form:

To obtain approval for driveway installation where driveway installation standards are required, the applicant must complete and sign a standard Driveway Application Form. The Driveway Application Form will require the applicant to furnish the following information:

- 2.2.1 Date of application
- 2.2.2 Name, address and phone number of applicant
- 2.2.3 Name, address and phone number of owner (if different from applicant)
- 2.2.4 Number and width of driveway entrances requested
- 2.2.5 Distance and direction of driveway (centerline) from lot corner
- 2.2.6 Elevation of property line in relation to edge of pavement at drive location (estimate)

2.2.7 Type of driveway entrance requested (culvert or dip-type)

2.2.8 Design size of culvert from approved subdivision drainage plan.
(City inspector will provide this information if not known by applicant)

2.2.9 Approximate date of driveway installation (if known)

2.3 Additional information and affidavit:

The application form will provide room for other information as necessary to describe the proposed facility and will contain space for a sketch, if necessary. The bottom portion will contain an affidavit form which must be signed by the applicant and which will indicate his agreement to construct the driveway in accordance with the standards, and to request inspection of culvert installation or ramp surface preparation at least 48 hours prior to pavement of the drive.

3. General Driveway Design Standards

3.1 Location:

No drive way shall be constructed within 150' of a signalized intersection or within the curb return of a street intersection or within the radius of the edge of pavement or traveled roadway at an intersection on a curve.

3.2 Width of Residence Driveways:

Minimum driveway pavement width on the public right-of-way for single family residences shall be twelve feet (12') with a maximum of thirty feet (30') and with fifteen feet (15') most desirable. Driveway base shall be 2' wider than pavement except for dip-type drives.

3.3 Width of Multi-Family and Commercial Driveways:

Multi-family residences and commercial uses shall have driveway pavement widths of twenty feet (20') minimum and forty-five (45') maximum, with thirty feet (30') most desirable and a twenty-four foot (24') minimum for two-way driveways.

3.4 Rdaii:

Driveway pavement radii shall be a minimum of five feet (5') into curbed streets and a minimum of ten feet (10') into uncurbed streets and shall be a maximum of fifteen feet (15') for either curbed or uncurbed driveways.

3.5 Common Drives:

Common driveways may be approved provided a permanent access easement has been granted to each property owner to use the portion of driveway on the other lot.

3.6 Number of Driveways:

On driveway access to any public road or street, a maximum of two (2) driveways will be permitted to any property with more than one hundred (100) feet or less, driveway access will be limited to one (1) access only. Where dip-type driveway installations are used (Sec 8.6.1 and Figure 8.1 and 8.2) two (2) driveways per lot will be allowed regardless of lot frontage.

3.7 Driveway Grades:

The maximum driveway grade for the portion of driveway constructed on public right-of-way shall be 10% (measured from the edge of shoulder) for residential driveways on local streets and 6% for commercial and industrial driveways. Multiple family driveways and commercial and industrial drives onto a street or road with a higher classification than a residential collector shall have a maximum grade of 3% for the first thirty feet (30') of the edge of the paved way of the street or road.

3.8 Variations:

Any variations from the above standards (i.e., a loading dock or special facility) must receive prior approval from the County Engineer.

3.9 Sight Distance Problems:

If sight distance problems are anticipated at the location of the proposed driveways, only one (1) driveway will be permitted at a site to be determined by the County Engineer or his representative that provides the safest access to the public right-of-way. Where alternate access is possible, access at hazardous locations may be prohibited.

4 Standards for Uncurbed Streets

4.1 Conveyance Standards:

Driveway installations requiring conveyance for storm drainage along roadside ditches shall be designed so as to provide adequate passage of the twenty-five (25) year local storm runoff for suburban and urban locations and ten (10) year local storm runoff for rural locations.

4.2 Overflow:

Additional provision shall be made for adequate overflow of storm runoff attributable to local storms in excess of the twenty-five (25) year or ten (10) year storm design frequency without damage to the adjacent road. Where culverts are used, pavement or riprap around culvert openings shall

be required as shown on standard details provided (see Figure 3-attached) unless roadside ditch grades are less than 5% or natural features will assure erosion of drive and road shoulder do not occur.

4.3 Culvert Pipe Length:

The length of culvert pipe where used shall be sufficient to allow for driveway base width (including radius as applicable) plus three times the pipe diameter plus three (3) feet, but in any case, no less than twenty feet (20'). The minimum pipe diameter allowed is eighteen inches (18") or Design 2 pipe.

4.4 Driveway Design Selection by City Engineer:

Driveway design for flood water conveyance will be as specified by the City Inspector with consideration of such factors as existing or proposed street drainage crossings, ditch shape, slope and subsoil type and location with relation to drainage breaks and/or culvert crossings. Should the applicant prefer to provide his own driveway design selection, such design and accompanying calculations shall be submitted by a Registered Professional Engineer.

4.5 Variances from Drainage Design Standards:

In subdivisions with roads constructed prior to January 1, 1980, where it is apparent that the existing roadside drainage system is inadequate to convey the required storm runoff and it is also apparent that normal roadside ditch maintenance will not allow improvement of the drainage system to convey the required storm runoff, the County Engineer may reduce the design requirements for pipe culverts or driveway dips to meet the available conveyance capacity.

5 Standards for Curbed Streets

Driveways cut into curbed streets shall be constructed to the general standards for driveway construction of the City of Mustang Ridge.

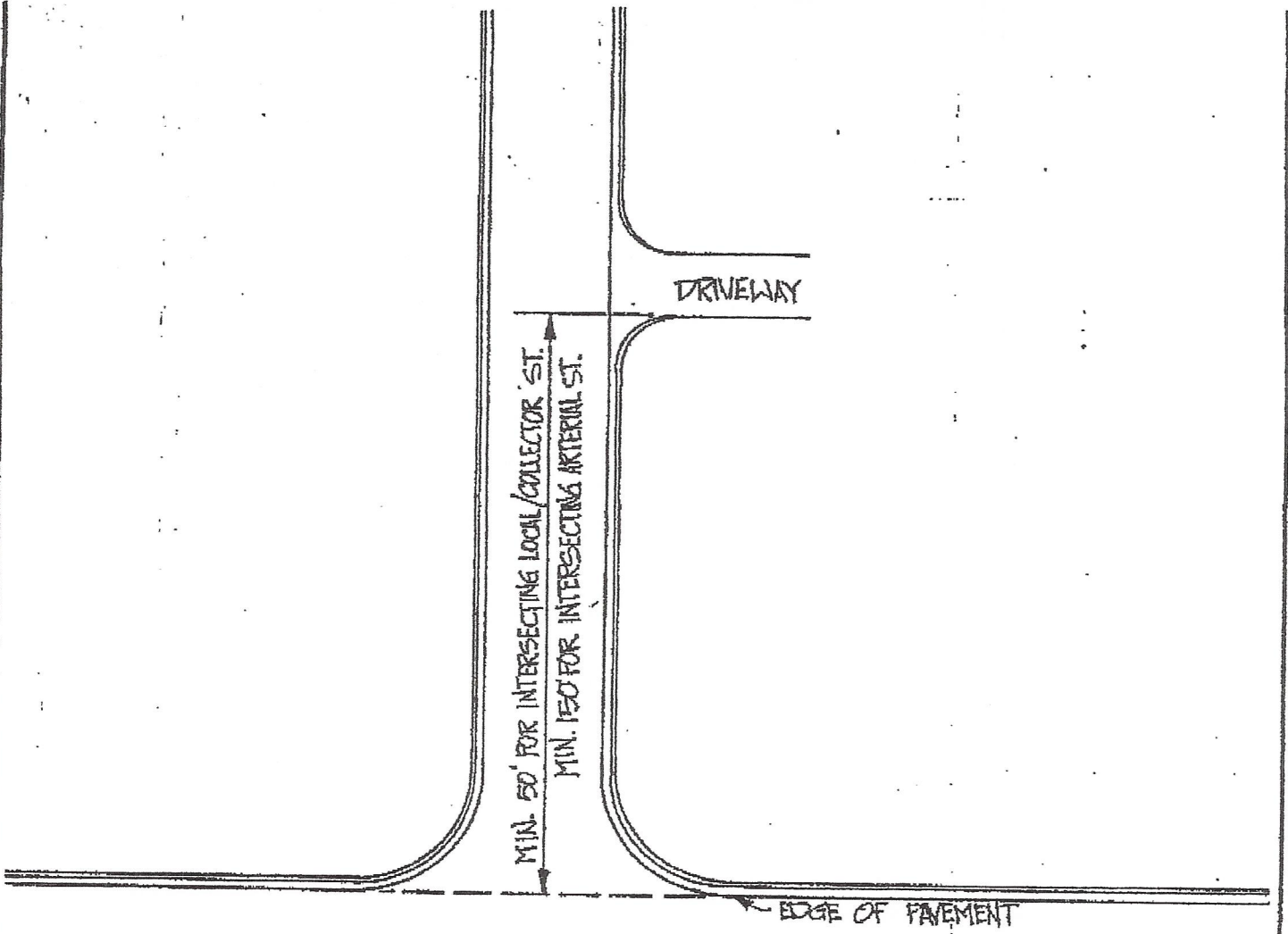
6 Driveway Installations - Uncurbed Streets

6.1 Dip-type Driveway Installations:

Properly designed and installed dip-type driveway installations function better to pass roadside drainage without scour damage to driveway or road shoulders or surface and are preferred where terrain will allow economical installation. Standard designs are provided in this section for both concrete and asphalt surfaces. (See Figure 1 and Figure 2 - attached). Installation of dip-type driveways approved under these standards will be inspected by the City Inspector for conformance with standard designs as applicable. Installations of dip-type driveways are not contemplated since complete installation can generally not be achieved within the public right-of-way.

6.2 Culvert Pipe driveway Installations:

Installation of culvert pipe driveway entrances approved under these standards and which is to property adjacent to accepted City roads, shall be under the supervision of the City Inspector??. The property owner, builder or subdivision developer shall install the driveway to meet these standards and as directed by the Inspector. No inspection or installation will commence until the property owner, builder or developer has an approved application and has presented same to the Inspector.



REVISION TO TRAVIS COUNTY STANDARDS
FOR CONSTRUCTION OF STREETS AND DRAINAGE IN SUBDIVISIONS:

~~SECTION~~ DRIVEWAY ENTRANCE STANDARDS

PARAGRAPH B.3.1. LOCATION:

No driveway shall be constructed closer than 150 feet to the edge of pavement of an intersecting arterial street. No driveway shall be constructed closer than 50 feet to the edge of pavement of an intersecting local or collector street.

DELETE NOTE NO. 8 OF APPENDIX D-1, PG. 2



TRAVIS COUNTY, TEXAS
PUBLIC IMPROVEMENTS and
TRANSPORTATION DEPARTMENT

TITLE: PARAGRAPH B.3.1. LOCATION
DRIVEWAY ENTRANCE STANDARD REVISION.

CONTRACT NO.

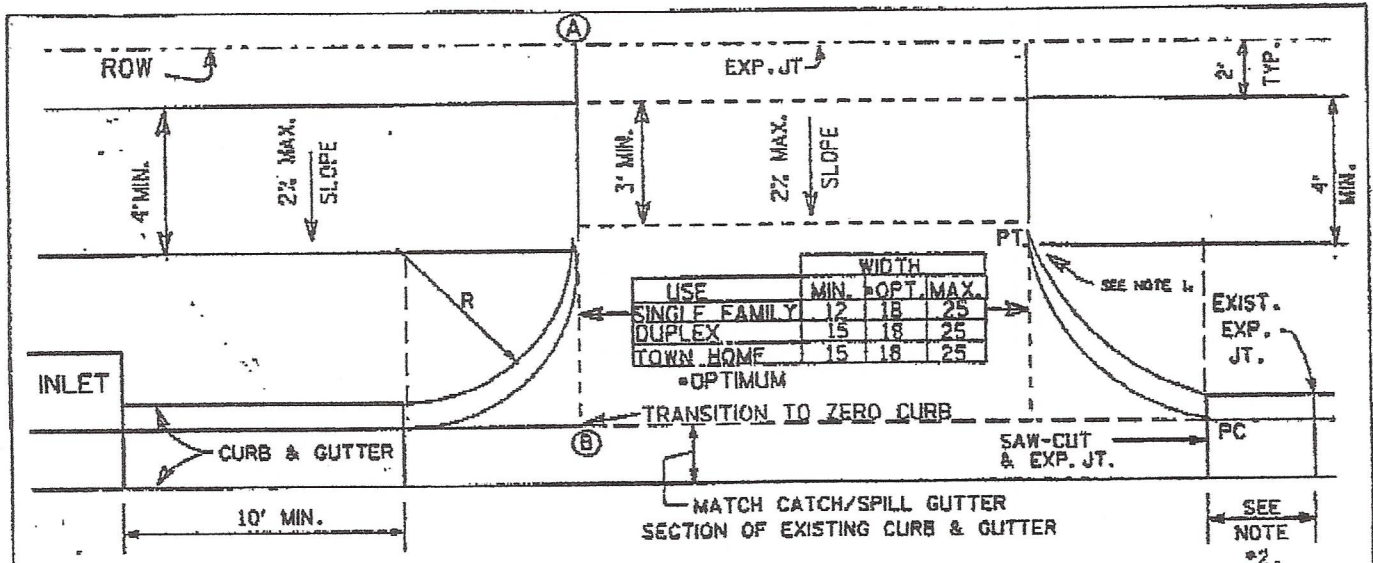
SHEET 1 OF 1
DATE: 1-23-91

DRAWN BY: BC

SCALE:

APPROVED BY:

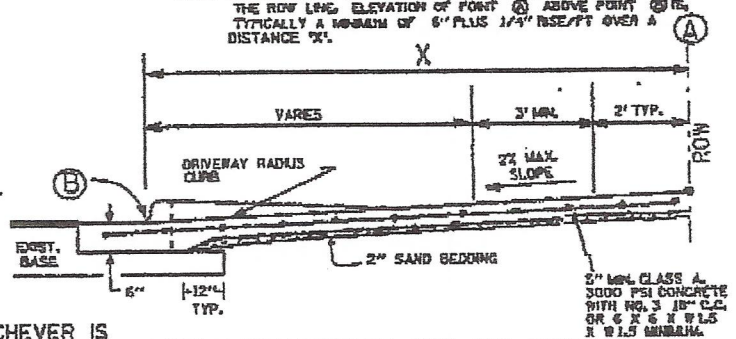
VERT. NTS
HORZ.



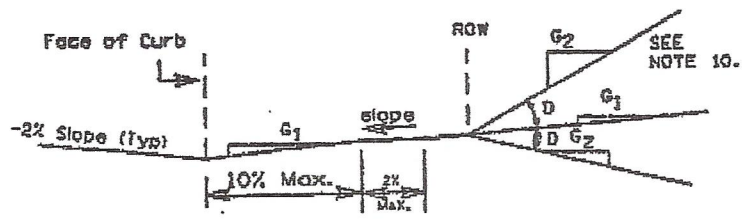
RADIUS DIMENSION

USE	MIN.	OPT.	MAX.
SINGLE FAMILY	5	5	10
DUPLEX	5	8	10
TOWN HOME	5	8	10

•OPTIMUM

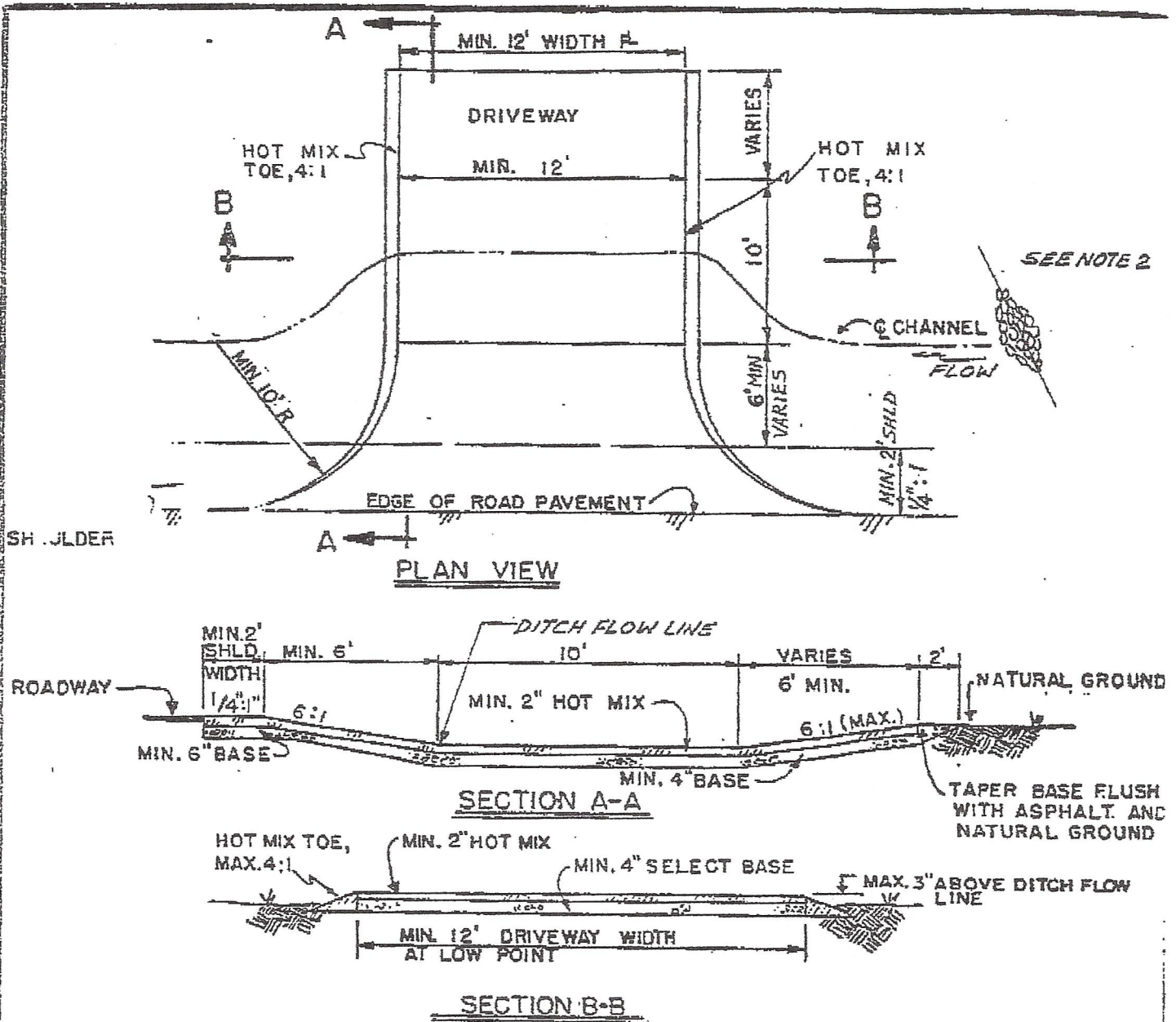


- 'ZERO' CURB AT PT OR SIDEWALK EDGE, WHICHEVER IS ENCOUNTERED FIRST. THE DRIVEWAY EDGE SHALL BE SMOOTHLY TRANSITIONED INTO THE SIDEWALK BEGINNING AT THE RADIUS PC LINE.
- IF DIMENSION IS LESS THAN FIVE FEET, REMOVE CURB & GUTTER TO EXISTING EXPANSION JOINT AND POUR MONOLITHICALLY WITH THE DRIVEWAY.
- IF THE BASE IS OVER-EXCAVATED WHERE THE CURB & GUTTER WAS REMOVED, BACKFILL WITH CONCRETE MONOLITHICALLY WITH THE DRIVEWAY.
- ALL DRIVEWAYS MUST BE CONSTRUCTED WITHIN THE STREET FRONTAGE OF THE SUBJECT PROPERTY AS DETERMINED BY EXTENDING THE SIDE PROPERTY LINES TO THE CURB.
- DRIVEWAYS SHALL NOT EXCEED 70 PERCENT OF A LOT'S STREET FRONTAGE.
- TYPE I DRIVEWAYS ARE TO BE LOCATED NO CLOSER TO THE CORNER OF INTERSECTING RIGHTS OF WAY THAN 60 PERCENT OF PARCEL FRONTAGE OR 50 FEET, WHICHEVER IS LESS.
- DRIVEWAYS SHALL NOT BE CONSTRUCTED WITHIN THE CURB RETURN OF A STREET INTERSECTION.
- SINGLE FAMILY LOTS LIMITED TO ONE DRIVEWAY EXCEPT FOR APPROVED SEMI-CIRCULAR DRIVES.
- WHEN TWO DRIVEWAYS ARE USED (ONE PER UNIT; TWO MAXIMUM) FOR DUPLEXES AND TOWNHOMES, SINGLE FAMILY STANDARDS SHALL APPLY.
- WHILE THE PROPERTY OWNER REMAINS RESPONSIBLE FOR GRADE BREAKS WITHIN PRIVATE PROPERTY, THE FIRE DEPARTMENT SHOULD BE CONSULTED WHERE THE DRIVEWAY IS ESSENTIAL TO EMERGENCY VEHICLE ACCESS AND 'G2' IS GREATER THAN 15%. 'G1' PLUS 'D' SHOULD NOT EXCEED 15%.
- SEE TRANSPORTATION CRITERIA MANUAL SECTION 5 FOR OTHER DRIVEWAY REQUIREMENTS.
- USE 1/2" ASPHALT BOARD, OR OTHER APPROVED MATERIAL, FOR CURB AND GUTTER EXPANSION JOINTS.



ALLOWABLE GRADES

REV. DATE	DESCRIPTION	INITIAL
STD. No. 433-1	ADOPTED 08/17/94	JJ
SCALE: N. T. S.		
CITY OF AUSTIN		
DEPARTMENT OF PUBLIC WORKS AND TRANSPORTATION		
TYPE I DRIVEWAY (1 & 2 FAMILY RESIDENTIAL USE ONLY)		
<i>Lawrence</i> APPROVED		9-13-94 DATE



SEE NOTE 2

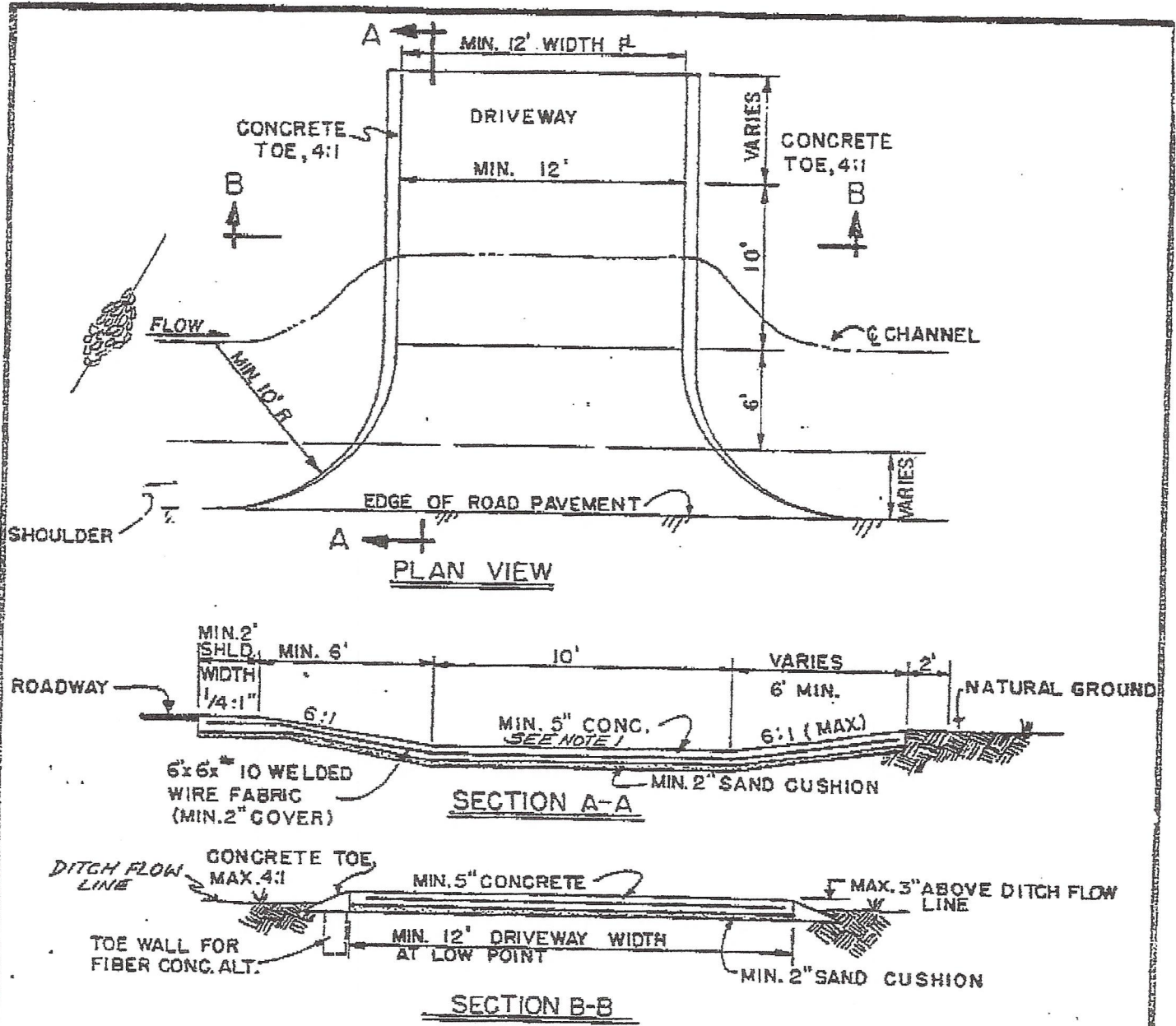
NOTES:

1. USE AS DESIGNED WITH DITCH GRADE FROM 0% TO 3%.
2. ADD DITCH RETARD AT 10' UPGRADE OF DRIVE WITH DITCH GRADE OF 4% TO 6%. SEE FIGURE 9.10.
3. USE CONCRETE DIP-TYPE DRIVEWAY AT DITCH GRADES OVER 6%.

ASPHALT DIP-TYPE DRIVEWAY

REVISED 9-6-83

FIGURE B.1



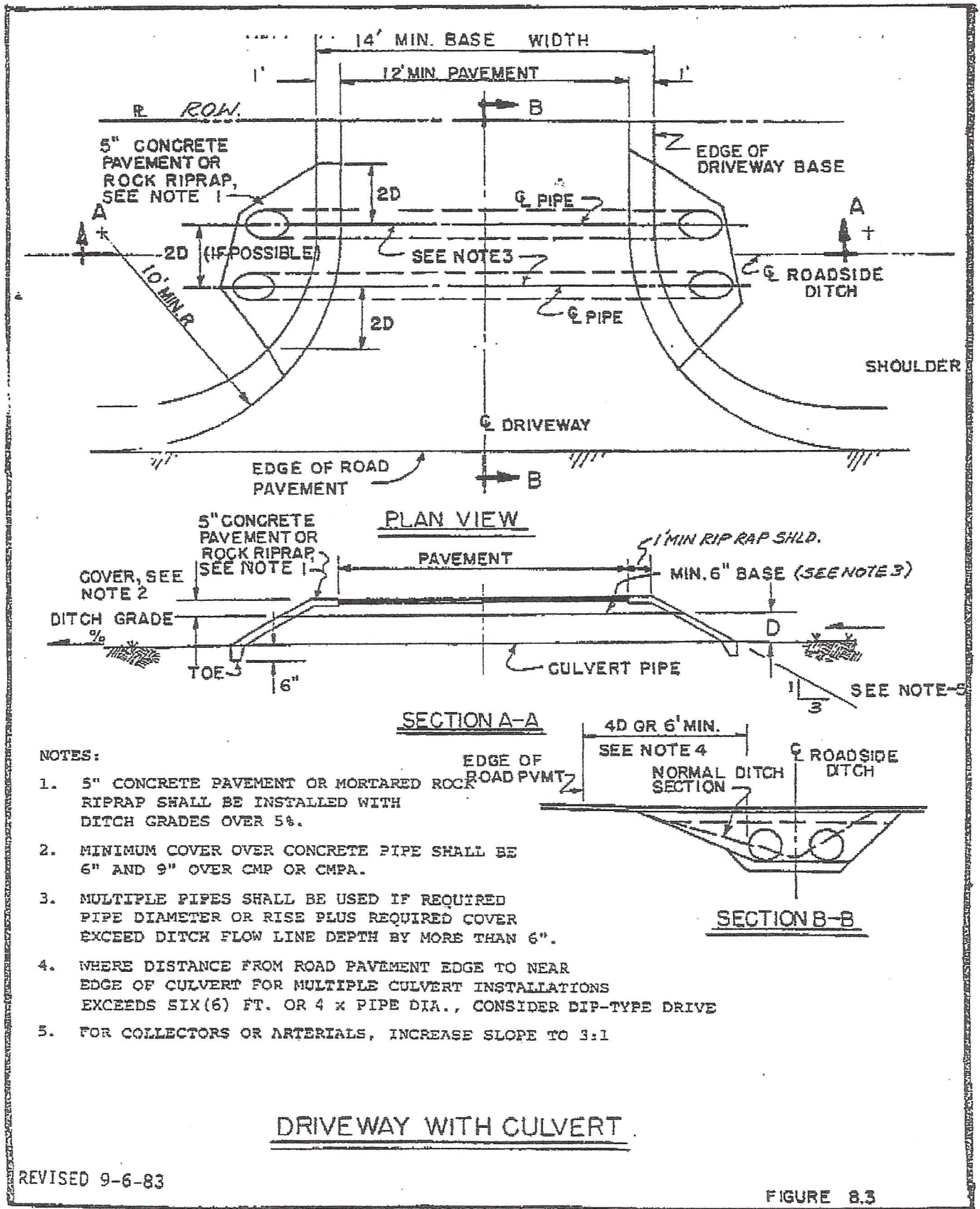
NOTES:

1. AN ACCEPTABLE ALTERNATIVE TO ABOVE IS 3" STEEL FIBER CONCRETE REINFORCED WITH FIBERS AS APPROVED BY THE COUNTY ENGINEER AND WITH 6" x 12" DEEP ANCHORED TOE WALL AND FULL SURFACE VIBRATION.
2. ADD DITCH RETARD 10' UPGRADE OF DRIVE WITH DITCH GRADE OVER 4% FOR BLACK SOILS AND OVER 8% FOR CALICHE SOILS. SEE FIGURE 9.10.

CONCRETE DIP-TYPE DRIVEWAY

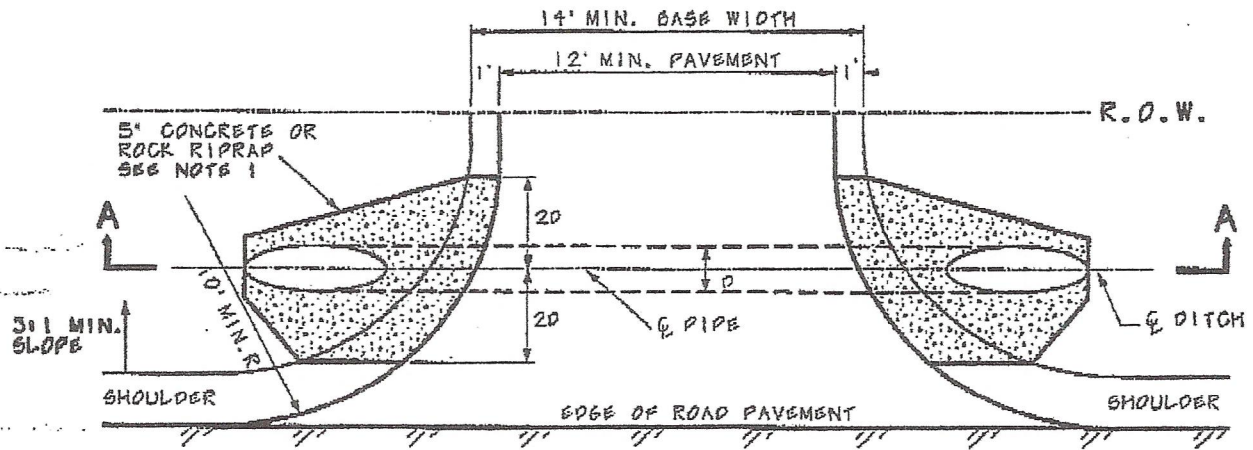
REVISED 9-6-83

FIGURE 8.2

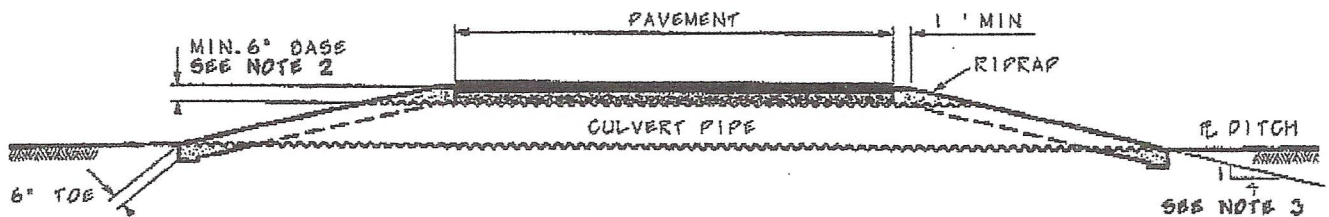


REVISED 9-6-83

FIGURE 8.3



PLAN VIEW

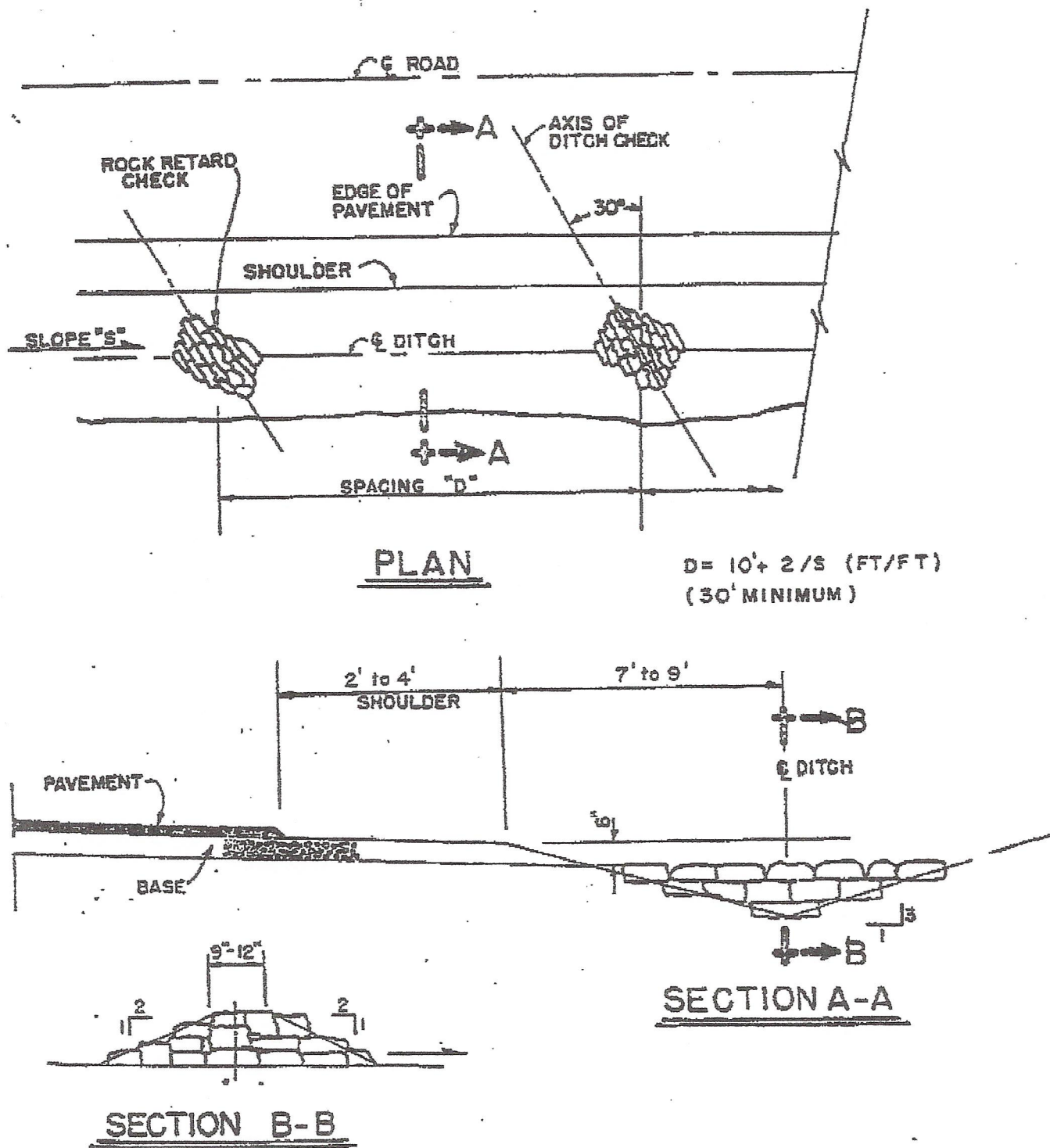


SECTION A-A

NOTES:

1. 5' CONCRETE OR ROCK RIPRAP SHALL BE INSTALLED WITH DITCH GRADES OVER 5%.
2. MINIMUM COVER OVER CONCRETE PIPE SHALL BE 6" AND 9" OVER CMP OR CMPA.
3. FOR COLLECTORS AND ARTERIALS, INCREASE SLOPE TO 6:1

DRIVEWAY WITH SINGLE CULVERT



TYPICAL ROCK RETARD CHECK

FIGURE 9.10